

**WA/2022/01984** – Outline application for up to 7 dwellings (no more than 1,000 sq m of floorspace) with vehicular access off Hookstile Lane, with some matters reserved (Landscaping). at UNITS 1 - 5 HOOKSTILE LANE FARNHAM GU9 8LG

Applicant: Mr Jon Boyes - Hookstile Properties Ltd  
Parish: Farnham  
Ward: Farnham Firgrove  
Grid Reference: E: 484100  
N: 146211  
Case Officer: Dylan Campbell  
Neighbour Notification Expiry Date: 29/05/2023  
Extended Expiry Date: 21/06/2023  
Committee Meeting Date: Western Planning Committee  
14/06/2023

RECOMMENDATION A That, subject to the completion of an appropriate legal agreement to secure the financial contributions towards the ongoing maintenance and enhancement of Farnham Park; and subject to conditions, that outline planning permission be **GRANTED**

RECOMMENDATION B That, in the event that a Section 106 Agreement is not completed within 6 months of the date of the resolution to grant permission, permission be **REFUSED**.

### Summary

This application has been brought before the Council's Planning Committee at a result of the call-in request from Cllr Hyman in order that Members can give due consideration to the issue of whether the changes to the housing scheme have overcome reasons for refusal relating to an earlier application for development on the site, being the safety of the access and effect on the Thames Basin Heaths SPA (WA/2020/0558).

This application follows application WA/2020/0558 which was allowed on appeal on 12 January 2023 for outline permission with all matters reserved for erection of 24 apartments.

This is an outline application with some matters reserved (landscaping) for the erection of 7 2-storey detached dwellings with associated parking and private amenity space with access onto Hookstile Lane comprising a 4.5m wide shared drive positioned towards the northeast of the site. As this is an Outline application,

landscaping would be subject of a future approval of reserved matters application. This application takes into consideration the access, appearance, layout and scale.

In light of the permission that was granted at appeal it is concluded that the principle of residential development of the site is acceptable and that the quantum of development can be achieved without harm to character of the area, of neighbouring amenities in terms of overlooking or an overbearing impact. The design is contemporary, amenity space for future occupants of the dwelling is limited, there are no highway safety issues. Whilst harm is identified in respect of the loss of employment land, this adverse impact is outweighed by the benefits of providing market housing -in a sustainably located site and on a brownfield site. The proposal is in accordance with Policies SP1, SP2, TD2, NE1, NE2, NE3, ICS1, AHN1, AHN3, LRC1, ALH1, ST1, CC1, CC2, CC3 and CC4 of the Local Plan (Part 1) 2018, Policies DM1, DM2, DM4, DM5, DM9, DM11 and DM13 of the Local Plan (Part 2) 2023 and FNP1, FNP12, FNP13, FNP15, FNP27, FNP30, FNP31 and FNP32 of the Farnham Neighbourhood Plan 2013-2032 (2020) and paragraphs 11(d), 126, 130, 167, 174, 183, 184 and 185 of the NPPF. On this basis, the recommendation is to approve the planning application, subject to completion of the legal agreement.

## 1. Site Description

The application site measures 0.23 hectares and is located to the south-west of Hookstile Lane, a single track private road accessed from Firgrove Hill to the east.

The site currently comprises dilapidated industrial buildings and an existing bungalow and associated parking space.

The site is relatively flat, stepped into the steep topography of the area which slopes up from north-west to south-east. Adjacent to the north-western boundary of the site, there is a railway line screened by mature vegetation. To the south-east and south-west there is an eclectic mix of residential dwellings along Arthur Road and Arthur Close. To the north-east there is a mix of residential and commercial buildings.

## 2. Proposal

Outline application for up to 7 two-storey, detached, three-bedroom dwellings (no more than 1,000 sq m of floorspace) with vehicular access off Hookstile Lane, with some matters reserved (Landscaping).

The dwellings would be contemporary in design and appearance, comprising, cladding, facing brick work and aluminium windows with a flat roof design to provide a large private amenity space on the roof terrace with a canopy over, topped with solar panels. Each dwelling would consist of a two double bedrooms, a study and bathroom on the ground floor and; a living, dining room and kitchen, utility room and a bedroom on the first floor, with access to additional private amenity space to the rear.

Each dwelling would also benefit from two off-road parking spaces.

### 3. Relevant Planning History

<b>Reference</b>	<b>Proposal</b>	<b>Decision</b>
PIP/2021/02768	Application for permission in principle for the erection of 9 dwellings.	REFUSED 28/03/2022
WA/2020/0558	Outline application with all matters reserved for erection of 24 apartments with underground parking following demolition of existing bungalow The Old Mission Hall (revision of WA/2018/1879) (as amplified by Transport Assessment, Travel Plan, updated ecology report, and amendment to proposed housing to include 7 of the 24 dwellings as affordable units).	REFUSE 28/07/2021 Appeal Allowed 12/01/2023
WA/2018/1879	Outline Application with all matters reserved for erection of 39 flats with underground parking following demolition of existing bungalow. (As amplified by drainage strategy received 18/12/2018).	REFUSE 01/02/2019

### 4. Planning Policy Constraints

Within the Built Up Area Boundary & Developed Area of Farnham  
Ancient Woodland 500m Buffer Zone  
Wealden Heaths I SPA 5km Buffer Zone  
Thames Basin Heath 5km SPA Buffer Zone  
AQMA Buffer Zone  
Potentially contaminated land  
Gas Pipe Line

### 5. Relevant Development Plan Policies and Guidance

- Waverley Borough Local Plan (Part 1): Strategic policies and sites (adopted February 2018): SP1, SP2, TD2, NE1, NE2, NE3, ICS1, AHN1, AHN3, LRC1, ALH1, ST1, CC1, CC2, CC3 and CC4.

- Farnham Neighbourhood Plan 2013-2032 (2020): FNP1, FNP12, FNP13, FNP15, FNP27, FNP30, FNP31 and FNP32.
- Waverley Borough Local Plan (Part 2): Site Allocations and Development Management Policies (adopted 21<sup>st</sup> March 2023) (LPP2): DM1, DM2, DM4, DM5, DM9, DM11 and DM13.

In accordance with the National Planning Policy Framework (NPPF) due weight has been given to the relevant policies in the above plans.

Other guidance:

- The National Planning Policy Framework 2021 (NPPF)
- National Planning Practice Guidance (2014)
- National Design Guide (2019)
- Land Availability Assessment (2016)
- West Surrey Strategic Housing Market Assessment (2015)
- Settlement Hierarchy (Update 2012)
- Open Space, Sport and Recreation (PPG17) Study 2012
- Statement of Community Involvement (2019 Revision)
- Strategic Flood Risk Assessment (2015/2016)
- Cycling Plan SPD (April 2005)
- Council's Parking Guidelines (2013)
- Residential Extensions SPD (2010)
- Vehicular and Cycle Parking Guidance (Surrey County Council 2018)
- Waverley Local Plan Strategic Highway Assessment (Surrey County Council, 2016)
- Employment Land Review (2016)
- Council's Economic Strategy 2015-2020
- Farnham Design Statement (2010)
- Climate Change and Sustainability Supplementary Planning Document (October 2022)

## 6. Consultations and Town/Parish Council Comments

Farnham Town Council	Objection – Limitations of the access cannot be overcome. Insufficient entrance for increased movements. The safety of pedestrians accessing the site cannot be achieved. Access in severely congested location. limited amenity space for future occupants. The site is better suited to its current use of light industry and should be classified as an employment site.
Council Environmental Health	No objection - Extensive potentially contaminative former light industrial / engineering uses over a number of years.

Due to these potentially contaminative historic activities, the introduction of residential use to the site and to ensure compliance with clause 183 and 184 of the NPPF. Recommend that contaminated land conditions are attached to the planning permission.

Surrey Highways Authority	Currently an objection and request for swept path analysis for the proposed new layout – SWEPT PATH PROVIDED AND AWAITING COMMENTS.
Surrey Wildlife Trust	No objection – The application should comply with the Wealden Heaths strategy; Recommendations and considerations given of protected species, along with sensitive lighting and ecology enhancements.
Natural England	No objection subject to securing appropriate mitigation for recreational pressure impacts on habitat sites (European sites).
Network Rail	No objection - Due to the close proximity of the proposed works to Network Rail's land and the operational railway, Network Rail requests that, where applicable, the applicant/developer follows the Asset Protection informatives which are issued to all proposals within close proximity to the railway.

## 7. Representations

20 letters have been received (from 18 households) raising objection on the following grounds:

- Previous refusal reasons not addressed.
- More homes not required.
- Overdevelopment.
- Loss of commercial units.
- No evidence to support change of use.
- Overlooking of neighbours and loss of privacy.
- Overbearing.
- Overshadowing.
- Loss of light.
- Inappropriate density and tightly packed.
- Limited amenity space for future occupants.
- Highways safety.

- Firgrove Hill/Hookstile Lane junction is significantly substandard in terms of its layout and visibility.
- Hookstile Lane is a substandard singletrack road with inadequate passing places and inadequate provision for pedestrians and cyclists.
- Shared surface route will not provide a safe environment for pedestrians and cyclists.
- Additional vehicle movements when occupied.
- Construction traffic generated by the development
- Layout has not demonstrated that a refuse vehicle can enter and leave the site in a forward gear.
- Lack of habitat assessment.

1 letter of support has been received outlining the following comments:

- Existing site is an eyesore.
- Considerable vehicle movements and traffic Monday – Saturday when operating as existing.
- If operated at full trading capacity, movements would be significantly higher.
- Visited by customers, deliveries, couriers, HGVs and lorries daily.
- No detriment based on historical uses of site.
- 7 houses will have lower vehicle impact than previous used as industrial.
- Benefit from resurfacing of access road.
- Contribution to the community compared to non-functioning estate.

## 8. Planning Considerations:

### 9. Principle of development

The principle of residential development and loss of employment land on this application site was established by the appeal allowed under WA/2020/0558 on 12 January 2023 granting outline permission with all matters reserved for the erection of 24 apartments.

This application seeks outline planning permission (landscaping reserved) for the construction of up to 7 detached two-storey dwellings.

In considering the current application officers have been mindful of any material changes in planning circumstances since the appeal decision in January 2023 including the adoption of the Local Plan Part 2 (2023).

Officers have concluded that there has not been a material change in circumstances since that appeal decision. However, significant weight must be given to the existing extant permission for residential development. It would be inappropriate and

unreasonable to revisit the principle of residential development in considering this application

#### 10. Planning history and differences with previous proposal

Planning application WA/2020/0558 sought outline permission (all matters reserved) for the construction of 24 flats. The application was recommended for approval with conditions subject to the completion of an appropriate legal agreement the recommendation was overturned by Planning Committee and planning permission was refused.

The main issues as amplified by the refusal reasons were:

- The effect of the proposal on highway safety with regards to access;
- The effect of the proposal on the living conditions of occupiers of neighbouring properties in respect of outlook and privacy;
- Whether the proposal would provide satisfactory living conditions for future occupants with regards to internal space, outlook and amenity space;
- The effect of the proposal on the character and appearance of the surrounding area;
- The effect of the proposal on the Thames Basins Heaths Special Protection Area and the Wealden Heaths Phase I Special Protection Area;
- Whether the proposal provides suitable affordable housing provision; and
- Whether the proposal provides appropriate play provision.

The test is whether having regard to the changes, the current proposal is materially more harmful than the approved scheme and that the scheme is acceptable in its own right.

#### 11. Housing Land Supply

The Council's current housing land supply is calculated to be 4.28 years' supply. As the Council cannot presently demonstrate a five-year housing land supply, paragraph 11(d) of the NPPF 2021 is engaged via footnote 7. Therefore, unless the site is located in an area, or involves an asset, of particular importance, that provides a clear reason for refusal, then permission must be granted unless it can be demonstrated that any adverse impacts demonstrably outweigh the benefits when assessed against the Framework as a whole.

The 'tilted balance' at paragraph 11(d) of the NPPF therefore applies and the development plan policies most important in the determination of the application must be considered out-of-date. Permission should be granted unless the adverse impacts would significantly and demonstrably outweigh the benefits.

The application proposes the erection of 7 dwellings. It would therefore result in a net gain of 7 residential units. The contribution of the site towards housing supply in the Borough is a material benefit to be weighed into the planning balance.

## 12. Housing Mix

Policy AHN3 of the Local Plan 2018 Part 1 states the proposals will be required to make provision for an appropriate range of different types and sizes of housing to meet the needs of the community, reflecting the most up to date evidence in the West Surrey Strategic Housing Market Assessment (SHMA). This Policy also states that the Council will support the provision of new housing and related accommodation to meet the needs of specific groups that have been identified in the SHMA. This currently, includes a specific need for families with children.

Policy FNP15 of the Farnham Neighbourhood Plan states that proposals for residential development on larger sites (0.2ha and above) will be permitted if they incorporate an element of 1 or 2 bedroom dwellings, subject to other policies in the Plan.

The proposal is solely for 3 bedroom dwellings and as such does not comply with the requirements of the Policy FNP15 of the FNP. The SHMA indicates that there is a specific need for families with children. This application seeks family sized dwellings and, in this respect, address a specific housing need within the Borough. . On balance therefore, given the need for housing in Waverley the proposed mix of housing is considered to be acceptable and no objection is raised.

## 13. Design and impact on visual amenity

Policy TD1 of the Local Plan (Part 1) 2018, Policies DM1 and DM4 of LPP2 and paragraphs 126 and 130 of the NPPF requires development to be of high quality design and to be well related in size, scale and character to its surroundings.

Policy FNP1 of the Farnham Neighbourhood Plan states that development will be permitted where it is designed to a high quality which responds to the heritage and distinctive character of the individual area of Farnham in which it is located. Guidance of the Farnham Design Statement should be followed.

The Farnham Design Statement 2010, with special reference to Firgrove, states that new development should reflect the distinctiveness of individual roads.

The site itself is regarded as Previously Developed Land, covered by hard standing, and comprising a small number of dilapidated industrial units. The site is at odds with the prevailing character of the area due to its industrial use situated within an otherwise residential area. Due to the small scale and single storey height of the existing industrial buildings, together with the set down of the site relative to



surrounding residential development, there is a very limited visual impact beyond the site boundary.

The application is submitted with landscaping reserved for future consideration. Therefore the proposed access, layout, scale and appearance of the scheme would be secured under this application. This is unlike the extant permission for 24 flats, which only included detailed matters of the approved access.

The relative height difference of the site ground levels compared to neighbouring properties (in particular Arthur Road) provides an opportunity to accommodate 2.5 – 3 storeys of built form without resulting in an over dominant impact on neighbouring street scenes or creating a form of development out of keeping with the area. The plans demonstrate that the proposed dwellings would be sited approx. 1.6m – 12.2m from the shared boundary with the dwellings on Arthur Road and due to the change in land levels the ridgeline of proposed dwellings would sit approx. 3m above the land level of Arthur Road. The proposed dwelling in the western part of the site would be located 3m from the shared boundary with Arthur Close and would not be directly opposite any existing rear elevation or habitable room windows to minimise any impact on neighbouring amenity.

The proposed dwellings would be staggered in a linear form with spacing between each of approx. 3.3m providing visual relief when viewed from neighbouring properties and gardens. Permeability of the site is limited, due to the single access in and out of the site. The site is otherwise landlocked, with residential properties and a railway line bordering the site. This creates a cul-de-sac form of development which goes against good design principles. Nonetheless, for a development of 7 dwellings, and given the existing characteristics of the site, this does not amount to significant harm in design terms, given the shape and size of the site there would be no alternative design solution.

The dwellings would be contemporary in design and appearance, comprising, cladding, facing brick work and aluminium windows with a flat roof design. The flat roof design is considered to be a creative way to provide a large private amenity space for each dwelling and also an opportunity to facilitate the use solar panels on the covered section of the roof terrace. Each dwelling would consist of two double bedrooms, a study and bathroom on the ground floor and; a living, dining room and kitchen, utility room and a bedroom on the first floor, with access to additional private amenity space to the rear.

Officers are satisfied that the site can facilitate 7 dwellings, parking, access and amenity space without resulting in overdevelopment, or harm to the character of the area. The contemporary design and appearance is considered acceptable and does not result in harm to the streetscene or visual amenity of the area in accordance with Policy TD1 of the Local Plan (Part 1) 2018, Policies DM1 and DM4 of LPP2, Policies H5, BE1 and FNP1 of the Neighbourhood Plan and paragraphs 126 and 130 of the NPPF.

#### 14. Impact on residential amenity

Policy TD1 of the Local Plan (Part 1) 2018 and Policy DM5 of the Local Plan Part 2 seeks to protect future and existing amenities for occupant and neighbours and ensure that new development is designed to create safe and attractive environments that meet the needs of users and incorporate the principles of sustainable development. Policy FNP1 of the Neighbourhood Plan relates to amenity. Paragraph 130(f) of the NPPF relates to amenity.

##### Neighbouring Residential Amenities

The proposed dwellings would run along the length of the application site and would front the proposed access road, with private gardens to the rear. The application site is bound by residential dwellings to the south on Arthur Road, which is at a higher land level and to the west by Arthur Close. The majority of dwellings along Arthur Road front onto the road, with private gardens to the rear. There are notable exceptions to this, numbers 2a and 6 are located to the back of other dwellings on the road and lie adjacent to the boundary with the application site. The gardens to these properties are more constrained than others on the road owing to their back land location. Along Arthur Road there is also a block of flats, Lamports Court. The flats, and the majority of dwellings are well set back from the site boundary by between 21-26m on the indicative plans. This is considered to be an acceptable distance to avoid adverse material impacts having regard to the form of the development proposed.

The proposed dwellings have been sited and designed as such that there would be no direct overlooking of the adjacent rear gardens of Nos. 2a and 6 Arthur Road due to the land level changes. This is demonstrated on the proposed Section BB plan, alongside the block plan and streetscene elevation.

To the west of the site, numbers 28 – 30 Arthur Close back onto the development site. There is a small landscape buffer area at that end of the site. The gardens of these properties are of a relatively modest depth, and as such only provide minimal separation to the site boundary. The building position is such that it is in closest proximity of number 30 of these dwellings. There is no significant change in levels between these properties and the application site. These properties only have very limited screening/protection from the existing industrial activities on the site. There are noise and privacy issues attached to this, particularly in terms of opportunities to enjoy the rear amenity spaces for these properties. A change of use of the site would in principle be more compatible with these residential uses.

One proposed dwelling would bound numbers 28 – 30 Arthur Close. The proposed dwelling would be orientated so that outlook would be directed towards its own private amenity space and towards the proposed access road. The first floor west facing side elevation window would also be conditioned to be obscure glazed.

Therefore, there would be no detrimental loss of privacy to dwellings on Arthur Close. It would be sited approx. 3m – 4m from the shared boundary with Arthur Close. It would also be sited forward of the rear elevation of 30 Arthur Close so that there would not be an overbearing impact or detrimental loss of outlook for existing occupiers.

Given the orientation of the application site in relation to neighbouring residential dwellings to the south and west, there would be no detrimental impact in terms of loss of daylight, or overshadowing.

### Future Occupier Amenities

The proposed dwelling has been considered against the technical housing standards - nationally described space standard dated March 2015 (the national standard). The dwelling would meet the minimum standard of 93m<sup>2</sup> for a three-bedroom, five-person dwelling over 2 floors as set out in the standard.

All bedrooms meet the national standards area and width requirements, and all habitable rooms would be provided with suitable outlook and privacy. Each dwelling would have an area of private amenity space to the rear of the property, however the majority of these would be relatively small areas. Therefore, each dwelling would also benefit from a generous private roof terrace which would measure approx. 8.4m in width and approx. 8.5m in depth (entire footprint of property).

In these circumstances, it is considered that the dwelling would benefit from adequate future occupier amenities and would not detrimentally impact neighbouring properties, in accordance with Policy TD1 of the Local Plan (Part 1) 2018, Policy DM5 of the Local Plan Part 2, Policy FNP1 of the Farnham Neighbourhood Plan and paragraph 130(f) of the NPPF.

## 15. Highway Safety

Policy ST1 of the Local Plan (Part 1) 2018 states that development schemes should be located where it is accessible by forms of travel other than by private car; should make necessary contributions to the improvement of existing and provision of new transport schemes and include measures to encourage non-car use. Development proposals should be consistent with the Surrey Local Transport Plan and objectives and actions within the Air Quality Action Plan. Provision for car parking should be incorporated into proposals and new and improved means of public access should be encouraged. Policy DM9 of the Local Plan (Part 2) 2023 relates to accessibility and transport and FNP30 of the Farnham Neighbourhood Plan relates to transport impact of development.

It is a material consideration that WA/2020/0558 was recently allowed on appeal for a larger development of 24 flats. The Inspector found that the visibility would be acceptable, and vehicles could enter and exit without having a detrimental effect on

highway safety; that the junction would not be hazardous to pedestrians; and the proposal would provide improvements to Hookstile Lane. The Inspector did note however that the proposal would provide substandard access along Hooksile Lane due to the lanes width and length, along with the proposed increase in the use of the lane, the proposed scheme would have an adverse effect on highway safety for users of Hookstile Lane, contrary to Policy ST1 of the LPP1 and FNP30 of the FNP.

However, in the planning balance, the Inspector attached minimal weight to the contributions to the local economy and employment opportunities and accessible location; and substantial weight to the provision of housing given the Council's lack of a five year housing land supply. The Inspector found that the proposal would not be harmful to living conditions of existing and future occupiers of surrounding properties and the proposed homes and it would not harm the character and appearance of the surrounding area.

On this basis, the Inspector found that the moderate adverse impacts of the development would not significantly and demonstrably outweigh the benefits and planning permission was granted.

As with the previous application WA/2020/0558, the third-party representations submitted in response to this application include a technical report on highway safety matters from the Farnham Firgrove Residents Association. The report finds that the proposed development would be unacceptable in highway safety terms and concludes that:

- the Firgrove Hill/Hookstile Lane junction is significantly substandard in terms of its layout and visibility. This will not change as a result of the proposed development.
- Hookstile Lane is a substandard singletrack road with inadequate passing places and inadequate provision for pedestrians and cyclists.
- the applicant's proposal to provide a shared surface route will not provide a safe environment for pedestrians and cyclists and the Firgrove Hill/Hookstile Lane junction will remain very substandard

#### Proposed Visibility Splays

Although the access onto Firgrove Hill is substandard with no kerb radii, it has been demonstrated that 2.4 x 43m visibility splays can be achieved to the nearside edge of carriageway in the leading direction and to the centre of the carriageway in the trailing direction. Visibility in the trailing direction is part obstructed by the railway bridge parapet. Taking this into account the visibility splay in the trailing direction has therefore been drawn to the centre of the carriageway, rather than to the nearside edge, which is considered acceptable given that this is an historic established access with no recorded accident history. This would be in accordance with Manual for Streets guidance for a road with a 30mph posted speed limit. Visibility in the trailing

direction (towards the railway bridge) should be maximised by the trimming back of vegetation along the site boundary to ensure there is no obstruction to visibility above 0.6m high. The submitted Transport Statement also demonstrates that there would be overall fewer vehicle trips over the course of the day compared to the existing lawful use. Furthermore, compared to the existing lawful use, there would be a reduction in the number of light and heavy goods vehicles using the access drive and pulling out at the junction. This would provide a safer environment for pedestrians and cyclists on Firgrove Hill. The County Highway Authority previously considered this to be a significant factor in assessing the highways impacts of the proposed development. A priority give-way system, with priority given to vehicles entering from Firgrove Hill, could be used to ensure that there would be no conflict arising from vehicles meeting on the access drive. These improvements, along with re-surfacing, could enable safe access to be provided for all road users and is therefore considered a betterment in road safety and sustainable travel terms.

The Inspector in relation to WA/2020/0558 also considered that given the width of the access at the HL/FH junction, the width of the footpath along with the location of the railway bridge brick pillar, street light and hedgerow, there is sufficient visibility for vehicle drivers exiting the HL/FH junction to observe oncoming vehicles approaching from the north and exit safely.

#### Proposed shared surface access

It is proposed that the access road will operate as a shared surface. This accords with the Surrey Design Guide from which it is acceptable for a shared surface access to serve a development of up to 50 dwellings where traffic speeds are less than 20mph.

The County Highway Authority have previously advised that they consider traffic speeds on the access road would be very low and less than 20mph. Concerns have been raised through the public consultation as to the width of the access road and the safety of pedestrians, including users with mobility impairments. In principle as a shared access the whole width of the access road would be available to pedestrians. However, to increase pedestrian visibility to drivers and perceptions of safety, and therefore encouragement to walk, a virtual footway has been proposed. This would provide an indicative area of pedestrian priority. The CHA has reviewed the width of the access road to ensure it is wide enough to allow vehicles to safely pass pedestrians. It was ascertained that with some trimming back of vegetation a width of 4m could be provided. However, 4m is not available for the entire length of the access road. The CHA measured the access at five different places using a topographical survey. This showed that it varies in width between 3.6 to 4.1m.

Allowing a standard 2.4m width for cars, the County Highway Authority has been satisfied that throughout the length of the access road the minimum recommended width for a footway of 1.2m would be available, allowing cars to safely pass pedestrians. Allowing the minimum 2.75m carriageway width for refuse vehicles and

fire tenders the available footway widths remaining for pedestrians would range from 0.85m to 1.35m. 1.2m would therefore not be available throughout the length of the access road. However, there would be sufficient places with the required width to allow safe refuge for pedestrians to wait should these larger vehicles be using the access road at the same time.

The width of the access road does not allow for a formal footway to be provided. However, a formal 1.5m or 2m wide footway would only be a requirement on busier routes with higher volumes of traffic, greater footfall and higher vehicle speeds. It is considered that, based on the Transport Assessment it is satisfied that the site would only be subject to a light level of traffic and that vehicle speeds would be low. The proposed solution of a shared access drive with a virtual footway is deemed acceptable in this location.

#### Refuse/emergency vehicle access

The information provided including swept path analysis has been assessed and it is considered that the access road provides the minimum required width of 2.75m to allow refuse vehicles and a fire tender to access the site and to be able to turn within the site so as to be able to access and egress in forward gear.

#### Sustainability of location

Given its proximity to Farnham town centre and to public transport facilities, the site is considered to a suitable location for residential development which provides an opportunity to maximise trips by sustainable modes of travel.

#### Car and cycle parking provision

The proposed plans show provision of 14 car parking spaces to serve the 7 dwellings. The Council's Parking Guidelines indicate a requirement of 2 spaces for each of the dwellings. This generates a total requirement of 14 car parking spaces, which the plans demonstrate could be provided on site. Any forthcoming permission would also include a condition relating to the submission and approval in writing of details of secure, covered facilities for the parking of bicycles.

#### Highway Improvements

The improvements to the access road shown/proposed all fall either within the application site boundary or within land owned by the County Highway Authority. The access improvements do not rely upon any alterations/use of the car parking spaces/land immediately to the front of Farnham Marble. This land falls outside of the application site area and is not relied upon for any improvements. In light of this it would be possible to impose conditions to secure the proposed access road improvements. Any other rights of access are a private matter.

The proposed development, subject to conditions, would accord with Policy ST1 of LPP1, Policy DM9 of LPP2 and Policy FNP30 of the Farnham Neighbourhood Plan and the NPPF.

## 16. Effect on the SPAs

Policy NE1 of the Local Plan (Part 1) 2018 states that the Council will seek to conserve and enhance biodiversity within Waverley. Development should retain, protect and enhance features of biodiversity and geological interest and ensure appropriate management of those features. Adverse impacts should be avoided or, if unavoidable, appropriately mitigated.

Policy NE3 of the Local Plan (Part 1) 2018 states that new residential development which is likely to have a significant adverse effect on the ecological integrity of Thames Basin Heaths Special Protection Area (SPA) will be required to demonstrate that adequate measures are put in place to avoid or mitigate any potential adverse effects. Such measures must be agreed with Natural England. New residential development which the Council considers that either alone or in combination is likely to have a significant adverse effect on the SPA beyond 400m and within 5 km of the SPA boundary (in a straight line) must provide:

- Appropriate contributions towards the provision of Suitable Alternative Natural Greenspace (SANG) identified by the Council;

Or

- A bespoke solution to provide adequate mitigation measures to avoid any potential adverse effects; and
- A financial contribution towards wider Strategic Access Management and Monitoring (SAMM)

The three tests as set out in Regulation 122(2) require S106 agreements to be:

- a) necessary to make the development acceptable in planning terms;
- b) directly related to the development; and
- c) fairly and reasonably related in scale and kind to the development.

The site is located within the 5km of the Thames Basin Heaths Special Protection Area (SPA). The proposal would result in an increase in people (permanently) on the site. In order for the development to be acceptable in planning terms, to comply with the Conservation of Habitats and Species Regulations 2017 and to avoid a likely significant effect upon the Thames Basin Heaths SPA a S106 agreement or Unilateral Undertaking is required as part of any subsequent planning approval to secure a financial contribution of £29,314.67 (£21,891.34 towards a SANG and £7423.33 towards SAMM), in line with the Waverley Borough Council Thames Basin Heaths

Special Protection Area (TBH SPA) Avoidance Strategy (July 2016). This figure is based on the proposal being 7 x 3-bed dwellings.

An Appropriate Assessment has been undertaken for the site. The Appropriate Assessment, which has been approved by Natural England, concludes that with appropriate mitigation, in this instance a financial contribution towards the SANG at Farnham Park, the proposal would not affect the integrity of the SPA.

Subject to the completion of an appropriate legal agreement, the effect upon the SPA would be mitigated in accordance with Policies NE1 and NE3 of the Local Plan 2018 (Part 1), Policy DM1 of the Local Plan (Part 2), Policy FNP12 of the Farnham Neighbourhood Plan 2017 and the adopted Avoidance Strategy.

#### 18. Biodiversity and compliance with Habitat Regulations 2017

Policy NE1 of the Local Plan (Part 1) 2018 and Policy DM1 of the Local Plan (Part 2) states that the Council will seek to conserve and enhance biodiversity. Development will be permitted provided it retains, protects and enhances biodiversity and ensures any negative impacts are avoided or, if unavoidable, mitigated.

The Wildlife and Countryside Act 1981 protects wildlife on development sites and confirms that it is an offence to injure, kill or disturb wildlife species and their nests or habitats. Further, Circular 06/2005 states 'It is essential that the presence or otherwise of protected species and the extent that they may be affected by the proposed development, is established before planning permission is granted.'

##### Badgers

The submitted ecology report has identified the likely absence of active badger setts within and adjacent to the development site. However, badgers may be present locally. The applicant should ensure that construction activities on site have regard to the potential presence of terrestrial mammals to ensure that these species do not become trapped in trenches, culverts or pipes. All trenches left open overnight should include a means of escape for any animals that may fall in. If badger activity is detected, works should cease and advice from a suitably experienced ecologist sought to prevent harm to this species. If any close-boarded fencing is to be used at the site, it is recommended that holes are included in the base of 20cmx20cm to allow badger to move freely through the site.

##### Bats

. The submitted ecology report is appropriate in scope and methodology and has identified the likely absence of active bat roosts within the development site. The bat emergence surveys date from 2020, but the update letter report confirms that conditions on the site remain the same in 2022. Therefore, bats do not appear to be present. However, bats are highly mobile and move roost sites frequently and



therefore should planning permission be granted the developer would need to follow good practice when undertaking the development Good practice should also be followed to protect any nesting birds.

### Great Crested Newts

The submitted ecology report is appropriate in scope and methodology and has identified the likely absence of great crested newt from the proposed development site. It would therefore appear that great crested newt is not a constraint to development. However, the applicant should be aware that suitable habitat for great crested newt exists within the development site and that should great crested newt be identified during works, all work should cease immediately and advice sought from Natural England or a qualified specialist.

### European Hedgehogs

Measures to enhance sites of European hedgehog should be incorporated into the proposal.

### Reptiles

The ecology report details a precautionary method of work which is considered appropriate in scope and content. A condition could be included in any permission to cover this matter.

### Sensitive Lighting

As outlined above bats do not appear to be present, however, bats are highly mobile and move roost sites frequently and therefore should planning permission be granted the developer would need to follow good practice when undertaking the development. Nocturnal species are sensitive to any increase in artificial lighting of their roosting and foraging places and commuting routes. Paragraph 185 of the NPPF (2021) states that planning policies and decisions should “limit the impact of light pollution from artificial light on ... dark landscapes and nature conservation.”

A condition is recommended for any forthcoming permission to ensure that the proposed development would result in no net increase in external artificial lighting on site boundaries at the development site, in order to comply with above referenced legislation and the recommendations in BCT & ILP (2018) Guidance Note 08/18. Bats and artificial lighting in the UK. Bats and the Built Environment. Bat Conservation Trust, London & Institution of Lighting Professionals, Rugby”.

### Biodiversity Enhancements

Policy DM1 of the LPP2 seeks to avoid negative impacts upon biodiversity deliver the minimum biodiversity net gain of 10% as required by the Environment Act 2021..

Although the submitted documents do not specifically outline a 10% BNG, SWT have advised that this development offers opportunities to restore or enhance biodiversity and such measures will assist the LPA in meeting the above obligation and also help offset any localised harm to biodiversity caused by the development process. SWT have recommended that should permission be forthcoming for the proposed development, the development should proceed only in accordance with the enhancement measures specified in Section 7 and Figure 2 of the Preliminary Roost Assessment and Phase 2 Emergence Survey Report. Therefore, no objections are raised regarding Policy NE1 of the LPP1, Policy DM1 of the LPP2 and paragraphs 174 and 180 of the NPPF.

## 19. Air quality

Paragraph 174 of the NPPF states that Planning policies and decisions should contribute to and enhance the natural and local environment by planning policies and decisions should contribute to and enhance the natural and local environment by preventing new and existing development from contributing to, being put at unacceptable risk from, or being adversely affected by, unacceptable levels of soil, air, water or noise pollution or land instability. Development should, wherever possible, help to improve local environmental conditions such as air and water quality, taking into account relevant information.

Policy DM1 of the LPP2 states that development should avoid significant harm to the health or amenity of occupants of nearby land and buildings, and future occupants of the development; and not cause a deterioration to the environment by virtue of potential pollution of air, soil or water.

The site is located in the AQMA buffer zone. Air pollution mitigation measures could be secured via condition should permission be granted. These include the submission and agreement of a Construction Environmental Management Plan, no burning of materials on site and the provision of electric vehicle charging points. Part S of the Building Regulation at 30 June 2022 required all new build homes and buildings to have electric car charging points therefore this is not a matter that needs to now be controlled through planning. Subject to the imposition of these conditions, it is concluded that the impact on air quality would be acceptable. The proposal would therefore accord with retained Policy DM1 of the LPP2 and the NPPF.

## 20. Contamination

Given the historic use of the site contamination can not be ruled out. to ensure compliance with Policy DM1 of the LPP2 and paragraphs 183 and 184 of the NPPF, any forthcoming planning permission would include a condition requiring a contaminated land investigation.

## 21. Flooding and Drainage

The site is located entirely within Flood Zone 1. It is, therefore, not necessary to consider the sequential or exception tests in this instance. Having regard to the above, it is considered that the proposals would not lead to increased flood risk, either on site or elsewhere, and would accord with Policy CC4 of the Local Plan (Part 1) 2018, Policy FNP31 of the Farnham Neighbourhood Plan and paragraph 167 NPPF in this respect.

## 22. Climate Change & Sustainability

Specific policies and the Council's Climate Change and Sustainability SPD (October 2022) is relevant.

A Climate Change and Sustainability Checklist has been submitted in support of the application. The proposals is sustainability located, close to public transport and local services. The dwellings would be constructed in sustainability source materials. It would include solar panels and battery storage, EV charging points, LED lighting and water butts. Water usage would also be conditioned as part of any forthcoming permission. These measures are considered proportionate to the scale of development proposed and would comply with the relevant policies and guidance. A condition requiring the submission and agreement of a scheme of climate change and sustainability measures is recommended.

## 23. Conclusion

This is an outline application with all matters other than landscaping reserved. It is concluded that the principle of residential development of the site is acceptable and that the quantum of development can be achieved in a satisfactory manner. It is considered that the proposal would respect the character of the area and it also avoids any significant harm to neighbouring amenities or highways safety. The proposal would also provide ecological enhancements in accordance with the submitted measures. The proposal would however result in some harm due to the loss of employment land. Whilst the proposal would not be entirely in accordance with the Development Plan, the adverse impact of the loss of employment land, is not of such significance, as to significantly and demonstrably outweigh the benefits of the proposal providing housing on previously developed land on a sustainable location within the Built Up Area Boundary.

### **Recommendation**

RECOMMENDATION A:

That, subject to the completion of an appropriate legal agreement to secure the financial contributions towards the ongoing maintenance and enhancement of

Farnham Park; and subject to conditions, that outline planning permission be GRANTED:

1. Condition:

Application for approval of the reserved matters shall be made to the local planning authority not later than 3 years from the date of this permission. The development hereby permitted shall take place not later than 2 years from the date of approval of the last of the reserved matters to be approved.

Reason:

To comply with the requirements of Town and Country Planning Act 1990 (as amended).

2. Condition:

The application for the details of the landscaping (hereinafter called “the reserved matters”) shall be submitted to and approved in writing by the Local Planning Authority before any development takes place and the development shall be carried out as approved.

Reason:

To comply with Section 92 of the Town and Country Planning Act 1990 (as amended).

3. Condition:

The plan numbers to which this permission relates are:

23HTE/P01 – Rev A – Proposed Plans  
23/HTE/P03 Rev D – Proposed Block Plan Ground Level  
23/THE/P02 Rev A – Proposed Elevations

The development shall be carried out in accordance with the approved plans. No material variation from these plans shall take place unless otherwise first agreed in writing with the Local Planning Authority.

Reason:

To provide a satisfactory standard of development in accordance with Policy SP1 of the Waverley Local Plan (Part 1) 2018.

4. Condition:

No development above ground floor slab level shall take place until details of the proposed external facing and roofing materials and all hard surfacing materials shall have been submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the

approved details.

Reason:

In the interest of the character and amenity of the area in accordance with Policy TD1 of the Local Plan 2018 (Part 1) and Policy DM4 of the Local Plan (Part 2) 2023.

5. Condition:

No development shall take place until a scheme for the protection of the retained trees has been submitted and approved in writing by the Local Planning Authority. The tree protection measures shall be implemented prior to the commencement of demolition and retained throughout the demolition and construction phase of the development.

Reason:

Required prior to the commencement of development to ensure no irreversible harm to the trees and to protect and enhance the appearance and character of the site and locality, in accordance with Policy NE2 of the Local Plan (Part 1) 2018, Policy DM11 of the Local Plan 2023 (Part 2) and paragraphs 174 and 180 of the NPPF.

6. Condition:

No demolition and/or development shall take place until a set of monitoring requirements has been submitted to Local Planning Authority for their approval, to include a. Name of appointed arboriculturist/representative responsible for site monitoring b. Report structure of the site monitoring of approved tree and ground protection measures c. Detail of Monitoring frequency throughout the demolition/construction period d. Detail the process to inform the Local Authority of AMS and TPP amendments. e. Detail the process to inform the Local Authority of remediation measures to rectify identified damage The development shall be carried out in accordance with the approved details.

Reason:

In the interests of the protection of the rooting areas of trees in the interests of the visual amenity and character of the area in accordance with Policies NE2 and TD1 of the of the Local Plan (Part 1) 2018 Policy DM11 of Local Plan (Part 2) 2023.

7. Condition:

No development shall take place, other than that required to be carried out as part of demolition or approved scheme of remediation, until the following has been submitted to and approved in writing by the Local Planning Authority:

a) An investigation and risk assessment, in accordance with a scheme to assess the nature and extent of any contamination on the site, whether or not it originates on the site. The investigation and risk assessment shall be

undertaken by a competent person as defined in Annex 2: Glossary of the NPPF.

b) If identified to be required, a detailed remediation scheme shall be prepared to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property. The scheme shall include:

- (i) All works to be undertaken
- (ii) Proposed remediation objectives and remediation criteria
- (iii) Timetable of works
- (iv) Site management procedures

The scheme shall ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation. The remediation works shall be carried out in strict accordance with the approved scheme. The Local Planning Authority shall be given two weeks written notification of commencement of the remediation scheme works.

Reason:

To ensure that land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policy DM1 of the Local Plan (Part 2) 2023.

8. Condition:

Upon completion of the approved remediation works, a verification report demonstrating the effectiveness of the approved remediation works carried out shall be completed in accordance with condition 8 and shall be submitted to the Local Planning authority for approval prior to occupation of the development.

Reason:

To ensure that land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policy DM1 of the Local Plan (Part 2) 2023.

9. Condition:

Following commencement of the development hereby approved, if unexpected contamination is found on site at any time, other than that identified in accordance with Condition 8, the Local Planning Authority shall be immediately notified in writing and all works shall be halted on the site. The following shall be submitted and approved in writing by the Local Planning Authority prior to the recommencement of works:

- a) An investigation and risk assessment, undertaken in the manner set out in Condition 8 of this permission.
- b) Where required, a remediation scheme in accordance with the requirements as set out in Condition 8.
- c) Following completion of approved remediation works, a verification report, in accordance with the requirements as set out in Condition 9

Reason:

To ensure that land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with Policy DM1 of the Local Plan (Part 2) 2023.

10. Condition:

No development shall take place until a Construction Transport Management Plan (CTMP) has been submitted to and approved in writing by the Local Planning Authority. The CTMP should include details of:

- (a) parking for vehicles of site personnel, operatives and visitors;
- (b) loading and unloading of plant and materials;
- (c) storage of plant and materials;
- (d) programme of works (including measures for traffic management);
- (e) HGV deliveries and hours of operation;
- (f) measures to prevent the deposit of materials on the highway;
- (g) on-site turning for construction vehicles;
- (h) measure to ensure access is retained for existing users of Hookstile Lane.

The development shall be carried out in accordance with the approved details.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway user, in accordance with Policy ST1 of Local Plan 2018 (Part 1).

11. Condition:

No development shall take place until a Construction Environmental Management Plan (CEMP) has been submitted to, and approved in writing by, the Local Planning Authority. The CEMP shall provide for:

- (a) An indicative programme for carrying out of the works
- (b) The arrangements for public consultation and liaison during the construction works
- (c) Measures to minimise the noise (including vibration) generated by the construction process to include hours of work, proposed method of piling for

foundations, the careful selection of plant and machinery and use of noise mitigation barrier(s)

(d) Details of any floodlighting, including location, height, type and direction of light sources and intensity of illumination

(e) the parking of vehicles of site operatives and visitors

(f) loading and unloading of plant and materials

(g) storage of plant and materials used in constructing the development

(h) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate

(i) wheel washing facilities

(j) measures to control the emission of dust and dirt during construction.

The approved CEMP shall be adhered to throughout the construction period.

Reason:

In the interest of neighbouring amenity in accordance with Policy DM5 of the Local Plan (Part 2) 2023.

12. Condition:

No development shall take place until a surface water drainage scheme that satisfies the SuDS Hierarchy and be compliant with the national Non- Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include:

(a) The results of infiltration testing completed in accordance with BRE Digest: 365 and confirmation of groundwater levels;

(b) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+40% allowance for climate change) storm events, during all stages of the development. If infiltration is deemed unfeasible, associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 1.3 l/s;

(c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.);

(d) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected;

(e) Details of drainage management responsibilities and maintenance regimes for the drainage system;

(f) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

The development shall be carried out in accordance with the approved details.

Reason:

To ensure the design meets the national Non-Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off site



such to accord with Policy CC4 of Local Plan (Part 1) 2018.

13. Condition:

The development hereby approved shall not be first occupied unless and until a verification report carried out by a qualified drainage engineer demonstrating the drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls) has been submitted to and approved by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Reason:

To ensure the design meets the national Non-Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off site such to accord with Policy CC4 of Local Plan (Part 1) 2018.

14. Condition:

The development hereby approved shall not be first occupied unless and until a scheme showing visibility splays at the junction with Hookstile Lane and Firgrove Hill has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details prior to the occupation of the development and thereafter permanently retained.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway user, in accordance with Policy ST1 of Local Plan 2018 (Part 1).

15. Condition:

The development hereby approved shall not be first occupied unless and until an access road improvement scheme, including a priority give way system and resurfacing, has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway user, in accordance with Policy ST1 of Local Plan 2018 (Part 1).

16. Condition:

The development hereby approved shall not be first occupied unless and until a scheme for pedestrian access to the site, to include a demarcated area for

pedestrians running from the entrance to the proposed dwellings as far as the boundary with the public highway, has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway user, in accordance with Policy ST1 of Local Plan 2018 (Part 1).

17. Condition:

The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans (Drawing No. 23/THE/P03 Rev D) for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason:

Required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with Policy ST1 of Waverley Borough Council's Local Plan 2018 and the NPPF 2021.

18. Condition:

The development hereby approved shall not be first occupied unless and until the secure, covered facilities for the parking of bicycles have been provided within the development site in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. Thereafter, the parking facilities shall be retained and maintained for their designated purpose.

Reason:

Required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with Policy ST1 of Waverley Borough Council's Local Plan 2018 and the NPPF 2021.

19. Condition:

The development hereby approved shall not be first occupied unless and until a Travel Plan in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, Surrey County Council's "Travel Plans Good Practice Guide", and in general accordance with the 'Heads of Travel Plan' document has been submitted to and approved in writing by the Local Planning Authority. The approved Travel Plan shall be implemented prior to first occupation of the development and for each and every subsequent occupation of the development, be thereafter maintained and developed to the satisfaction of the Local Planning Authority.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway user, in accordance with Policy ST1 of Local Plan 2018 (Part 1).

20. Condition:

Information Pack (STIP) in accordance with the sustainable development aims and objectives of the National Planning Policy Framework and Surrey County Council's Travel Plans Good Practice Guide for Developers has been submitted to and approved in writing by the Local Planning Authority. The STIP should include:

- Details of local public transport services and location of rail stations and local bus stops
- Details of local car club and lift sharing schemes
- Maps showing local walking and cycling routes and isochrone maps showing accessibility to public transport, schools and local community facilities
- Information to promote the take-up of sustainable travel.

The approved Sustainable Travel Information Pack shall be issued to the first-time occupier of each dwelling, prior to first occupation.

Reason:

In order that the development should not prejudice highway safety nor cause inconvenience to other highway user, in accordance with Policy ST1 of Local Plan 2018 (Part 1).

21. Condition:

No machinery or plant shall be operated, no demolition or construction processes carried out and no deliveries taken at or dispatched from the site except between the hours 08:00 – 18:00 Monday to Friday, 08:00 – 13:00 Saturday and not at any time on Sundays, Bank or Public Holidays.

Reason:

In the interests of neighbouring residential amenity, in accordance with Policy DM1 of the Local Plan (Part 2) 2023 and Policy TD1 of the Local Plan Part 1 2018.

22. Condition:

The development hereby approved shall not be first occupied unless and until a scheme to demonstrate that the external noise levels within external amenity areas, will conform to the "design criteria for external noise" upper guideline value of 55 dB LAeq,T, as specified within BS 8233: 2014, Guidance on Sound Insulation and Noise Reduction for Buildings. The work specified in the approved scheme shall then be carried out in accordance with the approved

details prior to occupation of the premises and be retained thereafter.

Reason:

To ensure a good standard of accommodation is provided for future occupants such to accord with Policy TD1 of the Local Plan 2018 (Part 1) and Policies DM1 and DM5 of the Local Plan (Part 2) 2023.

23. Condition:

The development hereby approved shall be implemented in accordance with the enhancement measures specified in Section 7 and Figure 2 of the Preliminary Roost Assessment and Phase 2 Emergence Survey Report, and shall be retained thereafter with the approved details.

Reason:

Required to ensure the long-term management of habitats, species and other biodiversity features in accordance with Policy NE1 of the Local Plan (Part 1) 2018 and Policy DM1 of the Local Plan (Part 2) 2023.

24. Condition:

The development hereby approved shall not be first occupied unless and until a detailed scheme of external lighting has been submitted to and approved in writing by the Local Planning Authority. The development should be carried out in accordance with the approved details.

Reason:

In order that the development should protect protected species in accordance with Policy NE1 of the Local Plan (Part 1) 2018.

25. Condition:

The development hereby approved shall be implemented in accordance with the precautionary method of working outlined in the Preliminary Ecological Appraisal and Bat Scoping Report (dated 12 June 2020).

Reason:

In order that the development should protect protected species in accordance with Policy NE1 of the Local Plan (Part 1) 2018.

26. Condition:

No development above ground floor slab level shall take place until a scheme of climate change and sustainability measures has been submitted and approved in writing by the local planning authority. The scheme shall have regard for the content of the Council's Climate Change and Sustainability SPD (2022). The development shall be carried out in accordance with the approved details.

Reason:

In the interests of achieving a high standard of sustainability in accordance with Policies CC1 and CC2 of the Local Plan (Part 1) 2018 and the Council's Climate Change and Sustainability SPD 2022.

27. Condition:

The development hereby approved shall not be first occupied unless and until the highest available speed broadband infrastructure has been installed and made available for use.

Reason:

To ensure sustainable construction and design in accordance with Policy CC2 of the Waverley Local Plan Part 1 (2018)

28. Condition:

The development hereby approved shall not be first occupied unless and until the dwellings have been completed such to meet the requirement for a maximum of 110 litres of water per person per day.

Reason:

To ensure sustainable construction and design in accordance with Policy CC2 of the Waverley Local Plan (Part 1) 2018.

**Informatives:**

1. Community Infrastructure Levy (CIL)- - The Waverley Borough Council CIL Charging Schedule came into effect from 1 March 2019. - - Where CIL liable development is approved in outline on or after 1 March 2019 the liability to pay CIL arises at the time when reserved matters approval is issued.- - For further information see our webpages ([www.waverley.gov.uk/CIL](http://www.waverley.gov.uk/CIL)) or contact [CIL@waverley.gov.uk](mailto:CIL@waverley.gov.uk)

2. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Electric Vehicle Charging Points shall be provided in accordance with the Surrey County Council Vehicular, Cycle and Electric Vehicle Parking Guidance for New Development 2022. Where undercover parking areas (multi-storey car parks, basement or undercroft parking) are proposed, the developer and LPA should liaise with Building Control Teams and the Local Fire Service to understand any additional requirements. If an active connection costs on average more than £3600 to install, the developer must provide cabling (defined as a 'cabled route' within the 2022 Building Regulations) and two formal quotes from the distribution network operator showing this.
3. The applicant is expected to ensure the safe operation of all construction traffic to prevent unnecessary disturbance obstruction and inconvenience to other highway users. Care should be taken to ensure that the waiting, parking, loading and unloading of construction vehicles does not hinder the free flow of any carriageway, footway, bridleway, footpath, cycle route, right of way or private driveway or entrance. The developer is also expected to require their contractors to sign up to the "Considerate Constructors Scheme" Code of Practice, ([www.ccscheme.org.uk](http://www.ccscheme.org.uk)) and to follow this throughout the period of construction within the site, and within adjacent areas such as on the adjoining public highway and other areas of public realm.
4. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

#### RECOMMEDATION B:

That, in the event that a Section 106 Agreement is not completed within 6 months of the date of the resolution to grant permission, permission be REFUSED.

# Annex A

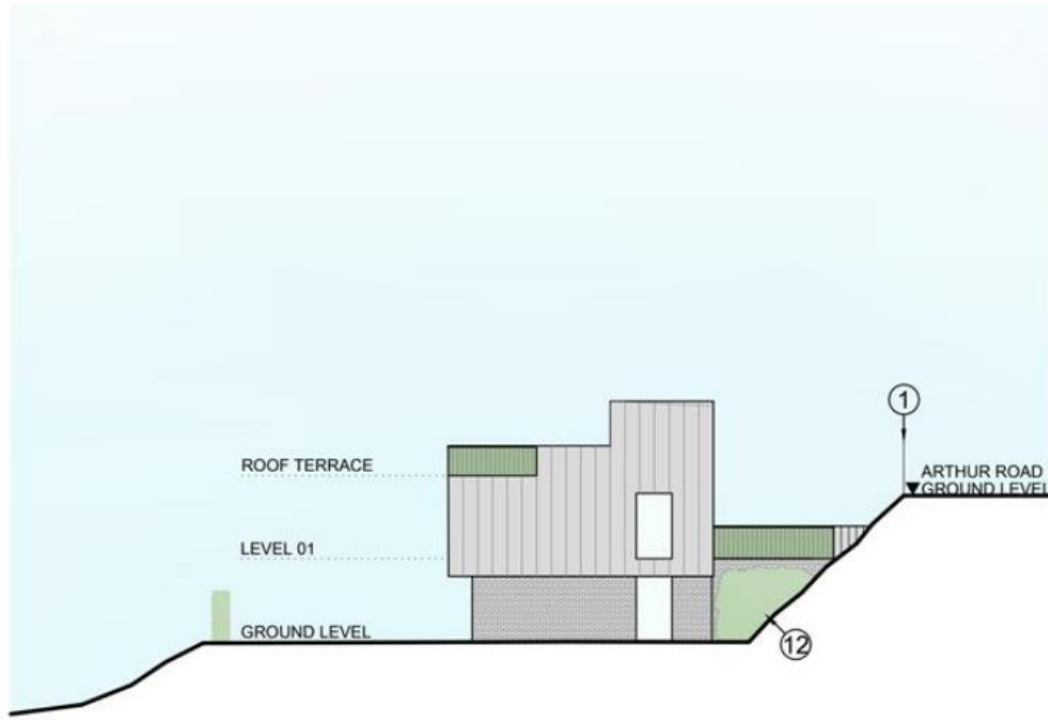
## Proposed Block Plan



## Proposed Streetscene



Proposed Section

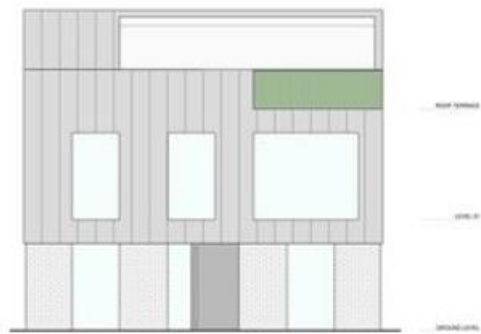


**Proposed - Section B - B**

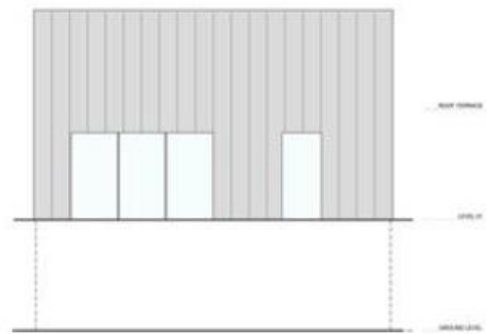
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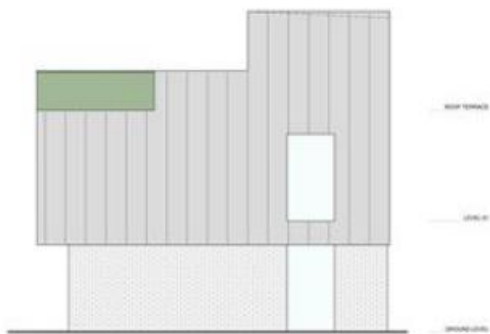
Proposed Elevations – House Type 2



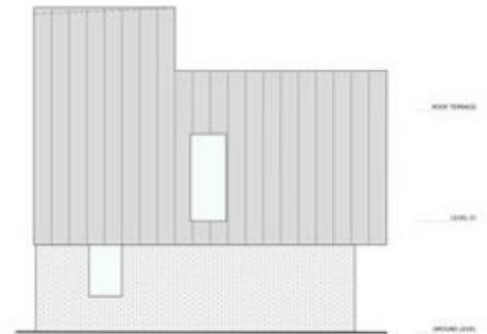
Proposed Front Elevation  
Scale 1 : 200 @ A1



Proposed Rear Elevation  
Scale 1 : 200 @ A1



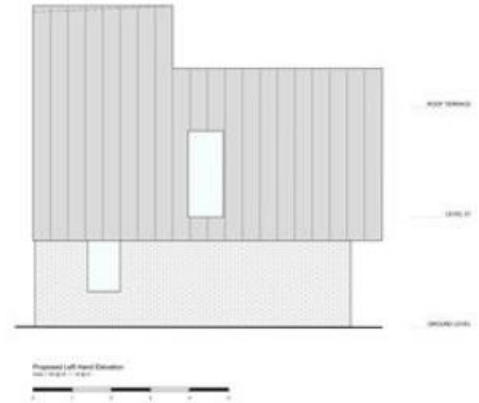
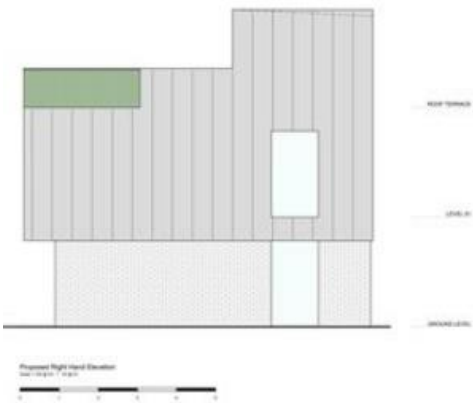
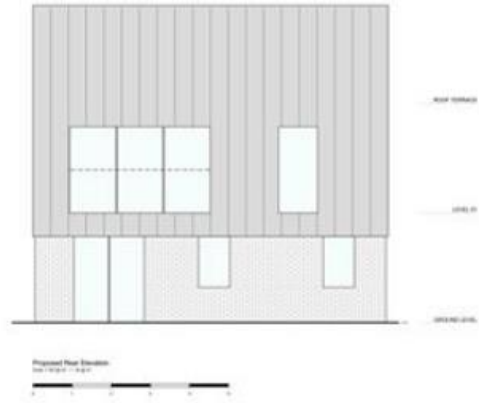
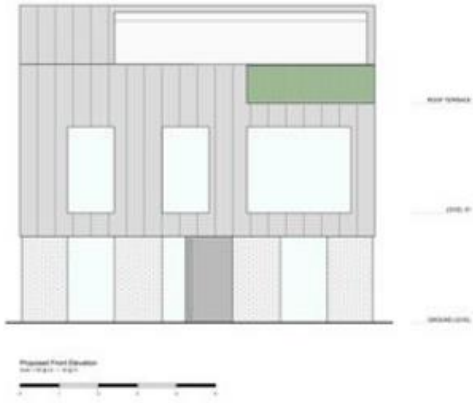
Proposed Right Hand Elevation  
Scale 1 : 200 @ A1



Proposed Left Hand Elevation  
Scale 1 : 200 @ A1



## Proposed Elevations – House Type 2



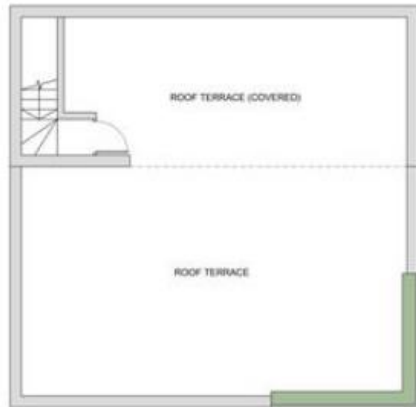
# Proposed Floor Plans House - Type 1



Proposed Ground Floor Plan  
Scale: 1:100 (A1) 1:100



Proposed First Floor Plan  
Scale: 1:100 (A1) 1:100



Proposed Roof Terrace  
Scale: 1:100 (A1) 1:100



Proposed Roof Plan  
Scale: 1:100 (A1) 1:100

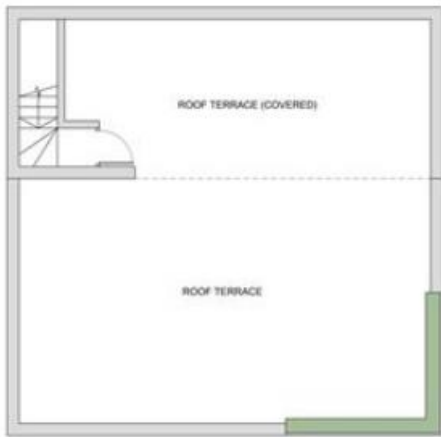
## Proposed Floor Plans - House Type 2



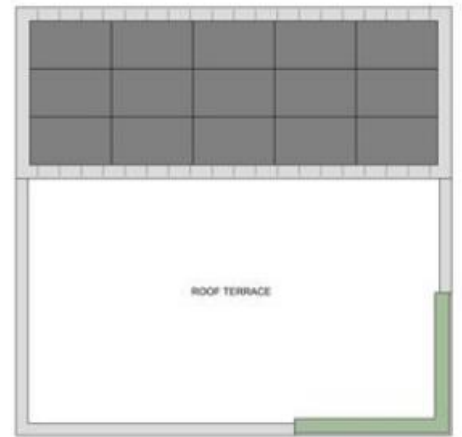
Proposed Ground Floor Plan



Proposed First Floor Plan



Proposed Roof Terrace



Proposed Roof Plan

